

THE BRASS POUNDER



Newsletter of the Carolina Southern Division 12, Mid-Eastern Region,
National Model Railroad Association

Volume 18 Number 4

June 2018

START PLANNING

As previously announced, CSD will host the MER Convention in 2020 ! Stay tuned for details as Convention Committees are formed and planning starts. Get ready to help CSD make it a great convention.

Division Coming Events

(See CSD Website for further details)

**CSD June Monthly Meeting
Saturday June 16th
10:00am - 2:00pm**

**Layout Visit at
Jack Monette's
174 Glenwood Dr. SW
Concord, NC 28025**

Superintendent's Corner

By Alan Hardee

Wow, it is June already. This month's column snuck up on me as our editor, Ed wanted to switch to monthly issues of the Brass Pounder. May was filled with many opportunities for Carolina Southern Division to showcase to the public. On May 5th, I spent the day at Duke Energy's Energy Explorium at McGuire Nuclear Station for their 30th Boy Scout encampment. I taught the Railroad Merit Badge to 22 Boy Scouts. The Scouts finished the day by building car kits furnished by Mid-Eastern Region that they got to take home. The North Carolina Transportation Museum in Spencer hosted National Train Day and their annual Train Show together on May 12th. Once again we were asked to provide clinics for the attendees to watch and learn new skills. Several CSD members volunteered time during the day to teach the clinics. Fred Miller, MMR, taught two clinics on

"Building Electronics Projects." Michele Chance taught a clinic on "Trees and Fauna" and "Buildings, Cardboard to Detailed." Mike Curtin had two clinics on "Making Trees with clippings from real trees and shrubs." Alan Burdick taught a clinic on "How to Solder." Bob Halsey gave his clinic on "Building and detailing basic car kits." Roy Becker was on hand as The Train Doctor to work on and/or test any items that the attendees may have. CSD was well represented in the layout department as well. All three model railroad clubs in our division with modules were on display in the Backshop area. Thanks to Central Carolina N-Scalers, Concord Area Model Railroad Club, and Metrolina Model Railroaders and their CSD members for their displays. I pulled multiple duties during this as the CSD display was setup, operating trains with Metrolina,

and taking photos. Metrolina and I represented the CSD during the 3 day Norfolk & Western Historical Society Convention (NWHS) at the NCTM May 17-19. Jim Wrinn, editor of *Trains* Magazine, was the guest speaker during the banquet dinner. He shared 40 photos from 40 years ago in Western NC taken with his first good camera. His best quote was, "Since being in Wisconsin for 12 years with *Trains* magazine, it's great to be back home where no one has an accent," as he is from the Franklin NC area. My friend and fellow CSD member Todd Arnett, whom some of you met in March, was elected President of the NWHS. Let's all wish Todd well in his new position. I used my time at both events to network with several clubs and layout owners about hosting tours and/or operating sessions during our 2020 convention.

Let us all congratulate Neal Anderson - Author, Gil Brauch – Structures, Bob Halsey – Volunteer, and Jack Monette – Volunteer as the latest CSD members earning AP Certificates. **Special congratulations** to Jack Monette, our Assistant Superintendent, as CSD's newest Master Model Railroader #613.

The June 16th CSD monthly meeting will be at the layout of Jack Monette, MMR. This is rescheduled from April when illness prevented our visit. Check the website for details and directions.

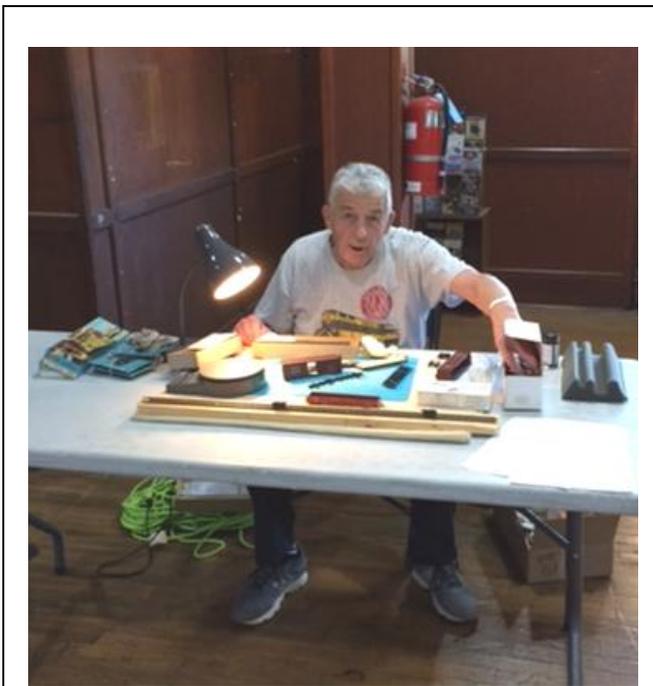


Figure 1 - Bob Halsey's clinic on Building and detailing a basic car kit.



Figure 2 - Metrolina Model Railroaders demonstrating a ScaleTrains.com preproduction C39-8 on their layout.



Figure 3 - N&W GP 9 #620 freshly repainted back to her original colors pulling a short train around the NCTM.



Figure 4 - Fred Miller, MMR teaching clinic on Building electronics Projects. Bob Halsey looking on.



Figure 5 - Neal Anderson's clinic on speed ballasting.



Figure 6 - Mike Curtin's clinic on making trees.



Figure 7 - A tree made during Mike Curtin's clinic.



Figure 8 - AP Chairman Dave Chance and Superintendent Alan Hardee presenting Neal Anderson with an AP Certificate for RR Author.



Figure 9 - Roy Becker working as "The Train Doctor."



Figure 10 - Norfolk & Western Historical Society posing with GP9 #620.

**UPCOMING
AREA TRAIN
EVENTS**

**Sipping & Switching
Society of NC Show
June 7th - 9th
Salem UM Church
378 N Pilot Knob Rd,
Denver, North Carolina
28037**

**1st Annual
[NC Rail-Strava-Ganza](#)
Train Show
Gastonia NC
Sat June 30-Sunday
July 1**

Editor's Notes

By Ed Gumphrey

This edition of *The Brass Pounder* marks a transition to monthly publication. Thank you for continuing to send me material for our newsletter. The bin of backlogs is growing, and if the pace continues, I'll be able to continue with the monthly publication.

In this issue you'll see a prelude to the June meeting at the home layout of Jack Monette, MMR, with some photos and information about his railroad from my visit there in March. You'll also enjoy the updates from Ed Smith and Tim Rumph on the continuing stories of their railroads under construction. I've also included some information from the clinics held at Spencer in May, and conclude this issue with the handout from Bob Halsey's clinic on improving freight car kits.

You might want to check out Sipping and Switching Society of NC at their Glenfest 2018 show at Salem United Methodist

Church in Denver, NC. They'll have a large modular display set up from 6:00pm Thursday, June 7th through 6:00pm Saturday, June 9th. Without the large traffic volume of typical of train shows, you can talk to members about their modeling techniques.

For members who may have attended the N&W Historical Society Convention or our neighboring Division, SER Convention in May, please share your convention experience with CSD by submitting an article for publication in a future edition of *The Brass Pounder*. I also welcome your submission on techniques and tricks you may want to share. I recently visited another model railroad in the area which will be featured in the coming months. If you'd like to see your railroad featured, let me know and I'll arrange a visit with you. Contact me by email to editor@carolinasouthern.org

DIVISION AND REGIONAL NEWS

By Ed Gumphrey

As Alan Hardee mentioned in *Superintendent's Corner*, CSD's next monthly meeting is a layout visit at the home of our newest MMR, Jack Monette. The meeting will be Saturday, June 16th from 10:00am until 2:00pm at 174 Glenwood Dr. SW, Concord, NC 28025.

Meeting Notes

By Michele Chance

CSD's monthly meeting for May was held at Spencer. Adding to Alan Hardee's comments about the meeting and successful clinics, I also received the following from Michele Chance about the clinics:

The clinics at Spencer Train Day were a joy. We had nice fellowship and a sharing of ideas. I enjoyed visiting with Fred Miller and seeing the additions to the electronic and animation controls. Bob Halsey also wowed us with adding good wheels to \$1 cars, the addition of dowel to allow for a screw and washer to make a swivel on the chassis seemed simple when you watched Bob in action.

My clinic was "Structures: Cardboard to Detail." I had visitors attend and talk with me, while I shared the joy of planning the scene by adding small box shapes collected from grocery goods such as tea boxes, protein bars, and raisin box. We talked about having square corners and simply cutting with scissors the shape of the foot print, even when cutting at an angle to meet the back drop. Next was the additions of construction paper, and if you really see the



Michele Chance presents her clinic on "Structures: Cardboard to Detail"
Photo provided by Neal Anderson

building taking a date way in the future, trying out prints from the internet to face the box with detailed window, doors, and features. Of course the last stage is to take plastic or wood and building up the structure you have planned from the cardboard mock up.

One visitor enjoyed my talk and show so much he gave me a ticket for a free ice cream. I enjoyed our audience and saw the love of trains and the family gathering to view the history of the mighty locomotives. Each visitor adds joy to our own love of trains, each of you brings new experiences and enthusiasm. See you at your layout, Michele Chance.

Achievement Program Update

Information provided by Dave Chance and Neal Anderson, CSD AP Co-Chairs

The following CSD members received AP Awards:

- Gil Brauch Structures
- Neal Anderson Author
- Jack Monette Volunteer

The CSD now has a new MMR as Jack Monette has completed his seven AP certificates for Master Model Railroader #613. Congratulations to Jack.

Please continue to let Neal Anderson know what AP goals you are working toward. Send him an email or give him a call.

kkrailroad@yahoo.com 704-775-2830

Jack Monette's Delaware & Seneca Railroad

By Ed Gumphrey

Jack Monette, MMR, has an interest in trains that goes back to his early childhood days. He first got into model railroading in the mid 50s, and as a child he enjoyed hanging out around the B&O roundhouse in Fairmont, West Virginia. Like most of us, his childhood interests were put on hold through various relocations (West Virginia, Ohio, Maryland, Pennsylvania), school, and early adulthood. He had a rekindled interest in the 70s, but didn't get seriously into the hobby until after he moved to North Carolina in 1982. This is his third iteration of his free-lanced railroad, heavily influenced by his early exposure to the Baltimore & Ohio, Monongahela, Buffalo Creek, and Chesapeake & Ohio Railroads.





Typical of shared trackage rights, a C&O GP pulls a string of scratchbuilt cars out of the main yard on Jack Monette's Delaware & Seneca Railroad



Home road motive power, D&S 1501 is a model of an uncommon center cab switcher. Ask Jack about the prototype when you visit.

Jack will tell you his real joy is scratchbuilding cars, and if you haven't seen it, you should sign up for his clinic, on Scratchbuilding 101 during the next RMU in January. In his clinic Jack makes a strong case for the enjoyment of scratchbuilding something besides structures. There will be an article about his approach in a future issue of *The Brass Pounder*.

Not limiting himself to building cars, Jack enjoys operation as well. To that end, Jack's railroad makes the most of a small space in a spare bedroom of his house, about 10' x 12'. Schematically, his track plan is an "out and back" arrangement. At first glance, the five laps

of a continuous perimeter helix around the room is a bit distracting, but as you focus on operation, the mechanics of reaching five different levels quickly fades. Using the popular car forwarding cards, and relying on the flexibility of DCC, Jack holds operating sessions on a fairly regular basis.

The Delaware & Seneca is a fictitious railroad, but the areas it connects are not. With the main yard area representing Baltimore, the railroad snakes around the room's multiple laps to connect with Pittsburg at its "western" end. A variety of industries provides opportunity for switching all types of cars. Whether you're spotting a boxcar at Jordan Mine Supply or a tank car at Potter Chemical, you'll stay engrossed with the operating potential of this compact but busy



Consolidated Coal No. 3 located at the west end of the D&S

model railroad. I'll let a few more photos tell the rest of the story and hopefully whet your appetite to see more at CSD's June monthly meeting which Jack is hosting. See the details in the bulletin above. You'll also be rewarded with seeing an interesting work station for scratchbuilding, and the chance to pick Jack's brain about some of his techniques.



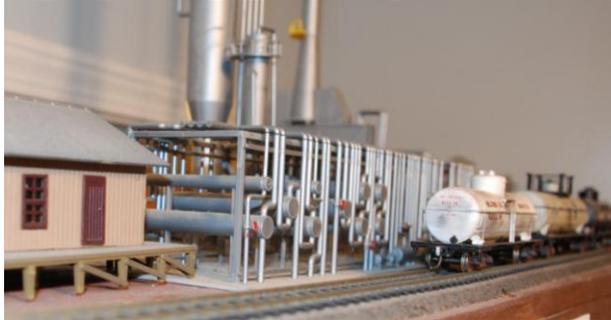
An overview of Jack's Delaware & Seneca main yard area.



Be prepared to see a variety of roads represented on the shared caboose track near the main yard.



Whether a local coal merchant or a campsite in the woods, there's plenty of trackside interest.



Chance Coal and Potter Chemical both see a lot of activity on the railroad.



Each lap around the railroad features different operating features.

Erie Railroad Delaware Division Part 2

By Ed Smith



My wife and I moved into our new home about 11 years ago. Needing a basement to build this empire, my wife found a one-acre sloping lot on Lake Hickory, very suitable for a walk-out basement. A 2500 sq. ft. home was built above my basement. My wife was in charge of everything upstairs and I had the basement.

As I stated earlier, the first 5 or 6 years were consumed with parental care and landscaping duties of our new lot. The last 5 years more time has been available for my one man crew, me, to work on the railroad. Much has been done. The luxury of building a new home and taking a few years before building the layout is that you have a clean space to work in and time to plan your project.

I had 2 major requests for our builder. First, we added 3 extra foundation block tiers, making the basement height just over 10 feet. Second, I wanted open space, no walls, so steel beams were installed for support, with just 1 support post (figure 1). Since I also had a 2500 sq. ft. foot print, I had the contractor frame in with studs, a bedroom, bath, living room/crew lounge, and utility room and shop on one side of the basement. The rest was wide open (figure 2).



Figure 1 - During construction, a crane sets supporting steel beams into place.



Figure 2 - A wide open basement area just waiting for a model railroad

With the house completed and the family moved in, I used my spare time over the next 6 years to finish the planned rooms, finish the train room, and layout the train plan (figure 3). Of all the building projects encountered, framing, sheet rock, plumbing, electrical, etc., the one that was most annoying was the drop ceiling. The small rooms went quickly, with the ceiling installed a little over 8 feet, allowing a 2 foot void above, for lighting and maintenance. This also created a void at the bottom of the stairs going down to the basement that I filled with the Erie logo. (figure 4) The large layout room, 1600 sq. ft., proved to be taxing for me, my 6 foot ladder, and 4 foot level. It took awhile, but I finally finished it. With the grid in, but no tiles installed, I had indoor/outdoor carpet installed to cushion the concrete floor.

It was time to sit down at my drawing table and plan my railroad. But I had to decide what part of the Erie railroad I would model. Throughout my whole life, I've been around the Erie. My Dad worked for the railroad, we took vacations on their trains, I lived in a city served by the railroad, and later in life, my wife and I still frequent the area when we visit the Finger Lakes in New York. But it wasn't until the NMRA National



Figure 3 - Finishing basement rooms gets closer to starting on the railroad.



Figure 4 - The Erie logo covers the ceiling void at the bottom of the stairs.

convention in 2006 in Philadelphia, that I made my decision. I visited Harold Werthwein's Erie RR Delaware Division layout in New Jersey, and I knew right away this was what I wanted to model. At my drawing table, I created a double deck, point to point, 2 track mainline connected by a helix. The decks would be at 37" at railhead on the lower level and 57" to 61" on the upper level (figure 5).

So with plan in hand, I went to the train room and used blue masking tape to lay out the aisles, minimum 30", and infrastructure, minimum 30" radii on curves. Once the tape was down, I installed CFL can flood lights down the aisles and filled in all the tiles.

It was time to build. I purchased a massive amount of lumber. Many 2x4x8's for legs and uprights, 1x4's and 1x2's for L girders, cross members, and rises, sheets of 1/2" and 3/4" plywood, and at least 50 sheets of 1/8" Masonite. Thank God for the walk-out basement. Delivery was a cinch. I let this wood sit in the train room for several weeks, while I built 8 bookcases to fit under the lower level (figure 6). Although I have a separate heat pump for the basement, I wanted the wood to adjust to the basement environment.

I spent a few days making L girders and moving the bookcases into place on the blue tape on the floor. It was time to build this thing. Using the 2x4x8's and L girders, I built the center spine of the layout using the tape as reference. This is where the 8' ceiling comes into play. Since it is a shade over 8', no cutting was needed for the vertical 2x4s. I added the 1x4 cross members at a height of 35" on the lower level and 56" on the upper level (figure 7). The risers and roadbed were added to get the appropriate height for each deck.

The three major yards and the two staging yards are a combination of 1/2" plywood and 1/2" homasote laminated with wood glue. Homasote was hard to find in North Carolina, but not impossible. The yards

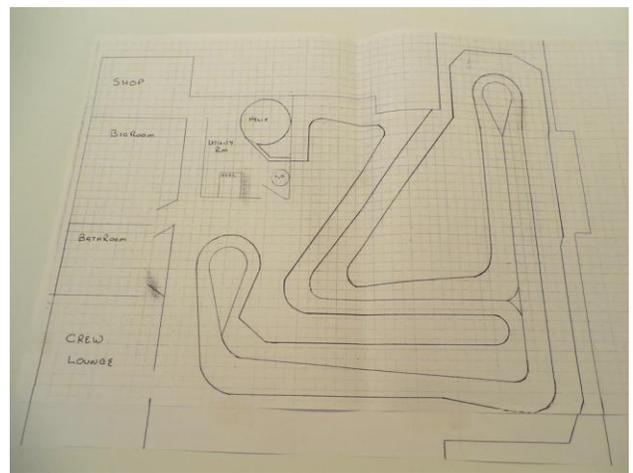


Figure 5 - The basic trackplan drawing set the stage for construction.



Figure 6 - Bookcases installed under the lower level.



Figure 7 - Layout framing of vertical 2x4s and 1x4 crossmembers.

are mounted on the cross members and risers at the proper height for the location. The staging yards are above each other and approximately 30' long. The lower yard is mounted on bookcases and the upper yard uses 12" right angle brackets mounted on the wall. (figure 8)

For my backdrop, I cut Masonite sheets into 3' widths and installed them on the upper deck, setting them on the crossmembers. I counter-sunk and screwed them into the 2x4 uprights. Once again, because of the 8' ceiling, these pieces fell just short of the ceiling. The pieces were butted together and all joints and screw holes were filled with drywall mud and sanded. The remaining 12" pieces of Masonite were used for the backdrop on the lower deck. Once all the Masonite sheets were installed and sanded, I painted them with cheap primer. (figure 9)

With this done, I made spline roadbed. First, I ripped the 1/8" Masonite sheets into 3/4" strips 8' long. Next, I ripped the 3/4" plywood into 1/2" strips 8' long. I cut these strips in 1 1/2" blocks. I laid out the route and placed risers where needed. The center block was drilled and a small nail secured the block to the riser allowing it to swivel. The 2 center strips were glued to the center blocks and they swivel to allow the spline to create radii required for curves with easements. (figure 10) Once installed, I just added the final 6 strips, 3 on each side, with blocks in between, staggered for support. It creates spline roadbed 4 5/8" in width, allowing me to apply 2 tracks of homabed at 2 1/2" centers. These strips were glued and clamped on the spline. Finally, after all glue dried, I sealed the homabed with cheap grey latex paint. (figure 11)



Figure 8 - Staging yards are on two levels.



Figure 9 - Backdrop materials attached to vertical 2x4s..

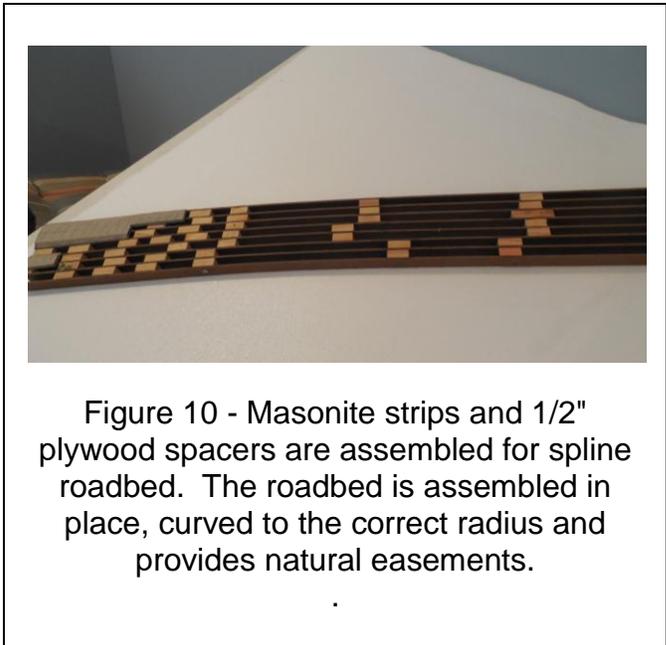


Figure 10 - Masonite strips and 1/2" plywood spacers are assembled for spline roadbed. The roadbed is assembled in place, curved to the correct radius and provides natural easements.

This entire process took considerable time, but it was worth it. I purchased a Craftsman compound miter saw and had access to a rip saw to cut the Masonite and plywood. These were life savers. Although time consuming, I found it relaxing and when finished, I had a feeling of accomplishment.

In my next update, I'll describe my decisions on the helix building, backdrops, both painting and using pre-printed scenes, fascia's, using Masonite and felt, and layout lighting. It's back to the layout room until then



Figure 11 - Spline roadbed provides smooth flowing curves with natural easements. It's time consuming, but worth the effort.



My Southern S-line Part 2

By Tim Rumph

Continuing from last month's introduction, plans for my model of the Southern S-line draws from a map of the eastern portion of the Southern Railway's S-Line. The modeled portions are shown in red on the map below (figure 1). The line starts in Salisbury and Spencer. These are represented by a staging yard, but I will have the wye in Salisbury for turning the Asheville Special passenger train, which Southern was still running three days per week in 1974.

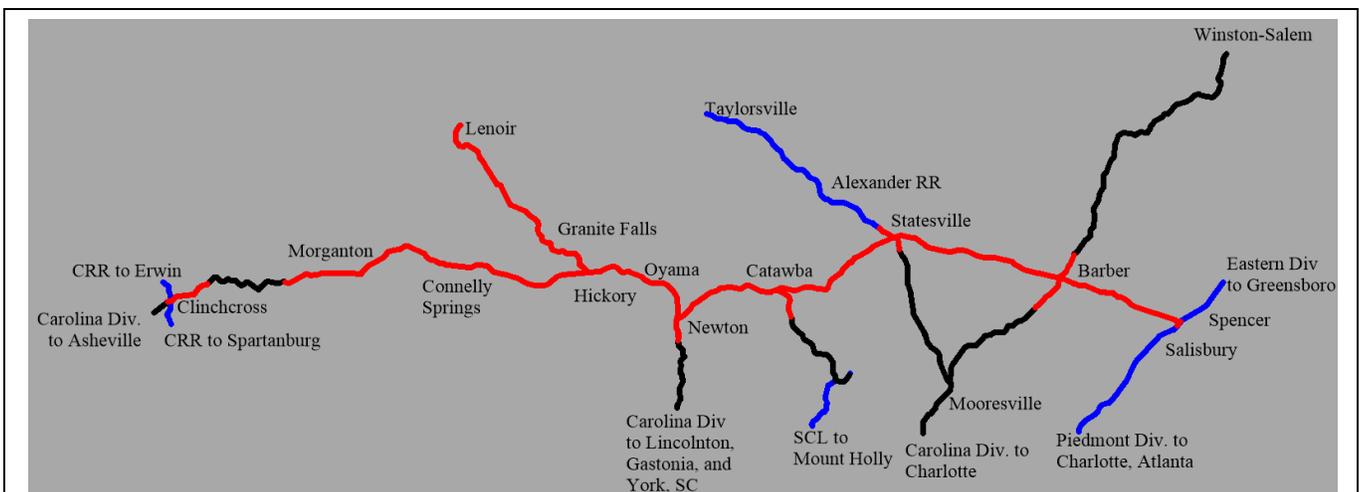


Figure 1 - A map of the Southern Railway's S-line, with the modeled portion shown in red.

The line leaves Spencer and is double track to Mojolica. The next station on the line going west is Barber. There isn't much at Barber, but it was an important junction between the S-Line and Southern's Charlotte to Winston-Salem line. The train from Winston-Salem would come down to Barber and give a block to train 163 on the S-Line, which was a first class merchandise train. This block would include carloads of cigarettes and the train would be inspected by special agents whenever it stopped to prevent theft.

Next came Statesville, which was a bustling town with furniture, textiles, agricultural, and machinery plants all served by the Southern. Southern's biggest customer here was the Statesville Flour Mill, which provided flour by the carload to large bakeries. It is now Bartlett

Mills, and is still served by Norfolk Southern. The interchange with the Alexander Railroad is also in Statesville.

After passing through Claremont the Southern reached Newton, where the Southern's Carolina and Northwestern line went south to Lincolnton, Gastonia, and York, represented by a small staging yard. Next was Conover and then Hickory, another large industrial town and where the Carolina and Northwestern went north to Lenoir.

There were several small mill towns between Hickory and Morganton. The biggest customer in Morganton was the Great Lakes Carbon plant, which made carbon black, a reinforcing material used for tire rubber and railroad brake shoes. It is now SGL Carbon and is still served by NS.

I skipped over the siding at Bridgewater to include the one at Clinchcross instead. It includes the interchange with the Clinchfield Railroad, on the east side of Marion. The track plan for the lower level of the layout, from Spencer to Newton, is shown below (Figure 2). Right now, I have the southern part of Newton installed, and the bechwork, track, and wiring is complete on that section, but the scenery is not yet started. I'm currently working on the Gastonia staging yard, and then will finish up Newton. The design for the upper level is still a work in progress.

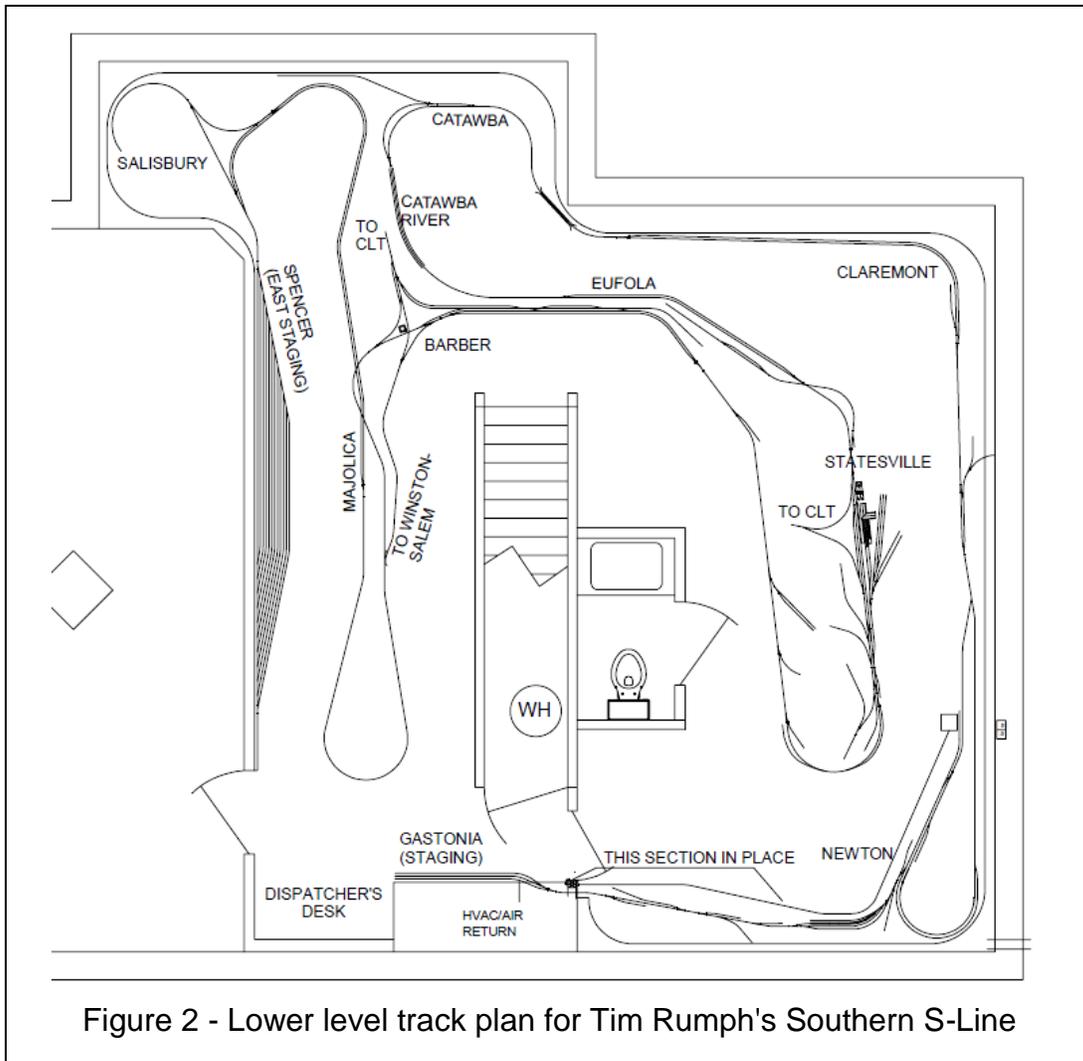


Figure 2 - Lower level track plan for Tim Rumph's Southern S-Line

Improving the Appearance and Performance of an HO Rail Car

By Bob Halsey

- 1) Paint underside frame, brake parts, outside of wheels, trucks, and couplers rust brown. Paint brake wheel same color as car body, floor boards weathered wood.
- 2) Weigh all the parts to determine amount of additional weight needed to conform closely to NMRA standards. The proper weight will allow the car to ride more smoothly and go through turnouts with less likelihood of derailing or "hopping".

Common prototype car lengths	HO length	HO car wt. (in ounces)
36'	5.0"	3.7
40'	5.5"	4
50'	6.9"	4.5
57'	7.9"	5
60'	8.3"	5
72'	9.9"	6
75'	10.4"	6.5
85'	11.7"	7

- 3) Add the necessary weights (1/4 oz increments) to inside floor of boxcar, reefer, or stock car, inside of hopper, gondola, or tank, and underside of flat (or as part of cargo load).
- 4) Assemble car. Adjust truck screws so that trucks turn freely and don't wobble. Usually one truck should be a bit looser than the other, to allow car to pass through curves.
- 5) Replace horn-hooks with scale couplers (Kadee or other). Using a coupler height gauge on your test track, check that coupler heights are correct. If necessary to adjust, add thin washers between truck and bolster, or inside of coupler box. Matching coupler heights helps avoid accidental uncoupling. Also check that trip pins clear the tops of rails. If not, either remove trip pins or bend them up slightly with trip pin pliers.

- 6) Spray the car with “Dull-Cote” to remove shiny plastic look. This is not always needed on newer manufactured cars. Apply weathering dust (lightly) to give used appearance, and paint rust streaks (if desired) or graffiti.
- 7) This is all you need for basic car improvement. The next step would be to remove the molded ladders, grab irons, and steps, replacing them with metal parts for a more realistic look. These are available from hobby shops and catalogs.
- 8) All the above steps also apply to passenger cars, but you must add car-end diaphragms, seats or dining car tables (if not already included), lighting, and, of course, passengers! Do very little weathering, since most railroads kept their passenger cars fairly clean!

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