

# THE BRASS POUNDER



Newsletter of the Carolina Southern Division 12, Mid-Eastern Region,  
National Model Railroad Association

Volume 18 Number 3

May 2018

## BREAKING NEWS

**As voted by the Board of Directors during the BOD meeting in April, CSD will host the MER Convention in 2020 ! Stay tuned for details as Convention Committees are formed and planning starts. Get ready to help CSD make it a great convention.**

### Division Coming Events

(See CSD Website for further details)

#### **CSD Monthly Meeting / Event May 12th**

9:00am - 4:00 pm In the  
BACKSHOP at N.C.  
Transportation Museum,  
411 S. Salisbury Ave,  
Spencer, NC 28159

*Note: Since the meeting and clinics are being held on May 12th, there will not be a monthly open house at Wade's Train World in May. Regular open houses will resume in June.*

#### **CSD June Monthly Meeting Saturday June 16th Location TBD**

## Superintendent's Corner

By Alan Hardee

It's hard to believe it's May already. Mother Nature sure is confused. We've gone from spring to fall and back again several times the last few weeks. Now that it's finally warm again, it's time to get out and enjoy some outdoor railroading. May is an important month for railroading as well. Do you know what happened on May 10th in Railroad history? The best known is probably the completion of the Transcontinental Railroad, with the pounding of the Golden Spike at Promontory, Utah on May 10, 1869. Railroads have changed a lot in the 149 years since. Next was the New York Central & Hudson River's steamer #999. It hit 112.5 MPH setting an unofficial speed record on May 10, 1893. This date is also relevant for model railroaders. William K. Walthers, the founder of today's Wm K. Walthers Inc. was born on this date in 1892 AND started his company on his birthday in 1932. I bet every one of us has something in our collection from his company.

National Train Day is also celebrated in May on the Saturday closest to the 10th. The North Carolina Transportation Museum (NCTM) will host a train show and NTD on the 12th. Carolina Southern Division will use this as our May Monthly Activity. We will be set up in the Backshop with our display and timesavers as well as having several How -To- Clinics during the day. We need several volunteers

during the day to help man the different stations so everyone has a chance to wander the Show. The Norfolk and Western Historical Society is having their 2018 Convention at the NCTM. The dates are Thursday May 17 through Saturday May 19. Visit their website, [www.nwhs.org](http://www.nwhs.org) for more details. CSD will have our display setup at the convention as well.

Speaking of conventions. The 2018 Crossroads of the MER Convention will be October 4-7 in Rockville Maryland. The convention website is <http://potomac-nmra.org/MER2018/> and has recently been updated with new details. Further down the road, CSD will be hosting the 2020 MER Convention. I'll have more information as we start planning for this event.

Carolina Southern Division 12 has voted to host the MER convention in 2020. That gives us two and a half years to form our committees and plan a great convention. Think about how you can help out. I'll be contacting everyone soon. We will also need plenty of layouts to visit and/or operate. So if you are like me and still need to finish your layout, let's get busy. Remember to take plenty of photos along the way as well. Our newsletter editor wants to post photos of layouts in progress and hopefully get more members in the AP program as well.

Congratulations to Jack Parker, MMR, on his Piedmont and Western Railroad in the May 2018 issue of *Model Railroader*. I have had the honor to operate on this wonderful layout. It is sure to be a hit for the 2020 convention.

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## Editor's Notes

By Ed Gumphrey

This is my third edition of *The Brass Pounder*. Thank you for the continued feedback I've received, and a special thanks for the many inputs for articles from CSD members. The bin of backlogs is growing, so I'll announce that I've reached my goal of graduating our newsletter from bi-monthly to a MONTHLY publication.

To grow our newsletter, I have enlisted the help of other members for regular inputs. Neal Anderson will be providing regular updates on members' progress in the Achievement Program. My budding friendship with both Neal and fellow AP Co-Chair Dave Chance has really opened my eyes to how the AP is structured in a way that provides positive reinforcement to helping you become a better model railroader. It's not just a program to provide recognition of your accomplishments. Similarly, other members have agreed to provide me with regular inputs to enable me to include several topics for each issue, to help put the "news" in our newsletter.

- Regular reports on CSD monthly and BOD meetings
- Updates on model railroads under construction
- Layout feature and project articles
- News about activities in the surrounding area

### **Other Regional Events**

**May 12**

**[Historic Spencer Shops Train Show](#)**

9:00am - 4:00 pm  
N.C. Transportation Museum, 411 S. Salisbury Ave, Spencer, NC 28159  
**Historic Spencer Shops Train Show**

**May 17-19**

**Norfolk & Western Historical Society  
[2018 Convention](#) in  
Spencer, NC**

**May 24-27**

**Southeast Region  
2018 Convention  
[Blue Ridge Rails](#)  
Lake Junaluska, NC**

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## DIVISION AND REGIONAL NEWS

By Ed Gumphrey

### UPCOMING AREA TRAIN SHOWS

**Model Train &  
Vintage Toy Show**  
[Greenville SC](#)  
May 19, 2018

**1st Annual  
[NC Rail-Strava-  
Ganza](#)  
Train Show**  
**Gastonia NC**  
**Sat June 30-  
Sunday July 1**

I am pleased to announce the news that Tim Rumph has answered the call from Superintendent Alan Hardee to act as CSD Program Director, not only for regular Division activities, but for our task hosting the 2020 MER Convention as well. Thanks in advance to Tim for volunteering his help.

CSD's next monthly meeting is less than two weeks away. Instead of observing our normal pattern of meeting the third Saturday of each month, we have shifted to scheduling alongside the events in Spencer on May 12th. I received the following information from Marcus Neubacher from the NC Transportation Museum (and also a CSD member):

*Hello everyone! This is a heads-up that the annual Historic Spencer Shops Train Show will be held on Saturday, May 12, at the North Carolina Transportation Museum in Spencer. Gil Brauch is helping with the planning, and I'd like to invite Alan Hardee and any others within the Division who would like to help out. In addition to spreading the word about vendors, we of course need layouts, clinics, and programs.*

*The show coincides with the NCTM Membership Day, National Train Day, and the Antique Truck Show. The best news is that this year we are not charging extra for the train show, so regular museum admission is all that is required.*

As I mentioned in the last edition, our regional organization needs help too. This issue of *The Brass Pounder* is the last before nominations are due for MER offices. For more specific information, and to submit nominations, contact Alan Hardee, our Superintendent [superintendent@carolinasouthern.org](mailto:superintendent@carolinasouthern.org) prior to May 30th.

As Alan noted above in *Superintendent's Corner*, the Norfolk and Western Historical Society [2018 Convention](#) is being held May 17-19 at Spencer. CSD will also have a display at the event, and Alan needs volunteers to help.

Also, don't miss out on our neighboring Region's annual convention. The Southeastern Region of MER will hold their annual convention, [Blue Ridge Rails](#) in nearby Lake Junaluska, NC May 24-27.

I know from talking with Alan Hardee that he is already making plans for this fall to attend the next MER Regional Convention [Crossroads of the MER](#) in Rockville Maryland October 4-7. This will be a great opportunity to not only experience the fun of a regional convention, but a good forum for learning how to help CSD host this annual event in 2020.

Beyond the fall (I know it seems a long way out, but time flies), start thinking about next year's Railroad Modeling University (RMU) event in CSD. There's more information below from Doug Algire. This past January was the first RMU for me, and I'm looking forward to the next one, with a goal of presenting a clinic in the future.

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# Meeting Notes

By Ed Gumphrey

## Saturday, March 17th CSD Monthly Meeting

15 CSD members (including new member Todd Arnett) attended the monthly meeting on March 17th. Key discussion included:

- Michele Chance gave a presentation on MER news and funding from MER for the Rail Pass trial memberships. CSD gained over 120 members, with over 80 more for MER. Michele is leading the effort to recoup over \$1000.00 to rebuild reserves after this significant expenditure. She also briefly discussed the upcoming MER Convention in October and general financial information about the relationship between regional and division costs and budgets.
- Gil Brauch provided data gleaned from questions submitted at January's RMU. Feedback remarks from those attending were very positive.
  - RMU extension will include clinics on trees and open car loads in the Spencer Backshop May 12th
  - Roy Becker will do Train Doctor during the event
  - Need volunteers for Brookford on June 9th
- Neal Anderson provided progress updates in the Achievement Program. See the details below in this edition.
  - 18 CSD members are actively participating in AP
  - Neal will provide monthly AP updates for The Brass Pounder
  - Recommend considering 3 of the easiest Certificates: Volunteer, Author and Dispatcher
- Seth Gartner brought along a coal tipple he recently completed. It was from an old kit he has had for many years. He also provided the photo of the tipple in place on his layout.



## Saturday, March 17th CSD Board Of Directors Meeting

Immediately following the membership's meeting, the BOD convened a meeting to discuss the question of whether CSD should offer to host the MER Annual Convention in 2020. Discussion included:

- Convention to be held in the greater Charlotte area
- Schedule for early October 2020, but not on a Charlotte race weekend



- Requirements covered by MER, such as negotiating Hotel contract, registration and contest arrangements
- Requirements covered by CSD, such as forming committees to arrange schedule, establish a website, non-rail activities, etc.
- There are significant volunteer requirements to meet Division responsibilities
- Is CSD ready to host this event?

The pros and cons were discussed at length. Interestingly, some of the most skeptical members ended up seeing the positive side of hosting the event. As Alan announced, the BOD voted unanimously to volunteer to host the convention. More details will be forthcoming.

### Saturday, April 21st CSD Monthly Meeting

The most recent meeting was held at Superintendent Alan Hardee's house. 9 members attended. A special welcome goes out to Fred Martin and Ed Smith for attending their first CSD events. We started with general discussions and a tour of Alan's model railroad. There will be more information on his railroad later in this edition. Engines had been pre-staged and sat idling and ready to run, but the discussions took over and we didn't actually do any operating.

After about an hour of gab, the group moved to Alan's screened porch to focus discussion on Division business, specifically the requirements for hosting the 2020 MER Convention and AP progress.

On the topic of the 2020 Convention, discussion focused on

- Ideas for the best location with the Hilton at University Place emerging as a likely favorite.

Factors include:

- Number of rooms needed
  - Space for contests and clinics
  - On-site restaurant
  - Availability of Audio-Visual support for clinics
- The geographic footprint of the MER and where most out-of-state visitors would likely be traveling from
  - Consideration of other locations may hinge on visitors' need for airport shuttle service
  - Committee needs
    - Committee chair or possibly 2 co-chairs
    - Layout Tour, Clinic and Advertising Chairs
    - Webmaster for dedicated convention website
    - Treasurer separate from Division Treasurer
    - Contest chair provided by the MER
  - Decisions will be needed on
    - Whether to hold an auction
    - Option of white elephant sale



Norfolk Southern 3646 sits idling on Alan Hardee's railroad, waiting for the operating session that got overtaken by conversations.

- Determination of non-rail activities
- More information to be determined and provided to the Division as planning proceeds.

Neal Anderson and Dave Chance provided discussion and information about the Achievement Program. Neal's emphasis on how the AP is structured was well-received, as was Dave's discussion of the path to MMR starting with The Golden Spike Award. Neal also presented Bob Halsey with a Certificate of Achievement for Association Volunteer. Congratulations to Bob on this milestone toward MMR Certification.

With business discussion completed, we enjoyed a round robin discussion introducing ourselves and talking about our model railroad interests. Some new friendships were formed, and there was a lot of enthusiasm about participating in both CSD events and the AP. I left the meeting feeling confident that our newcomers will become familiar faces at upcoming events.



Neal Anderson presents Bob Halsey with his Certificate of Achievement for Association Volunteer



During discussion about the AP, Dave Chance looks up the requirements for The Golden Spike Award on his cell phone as Fred Martin and Alan Hardee look on.



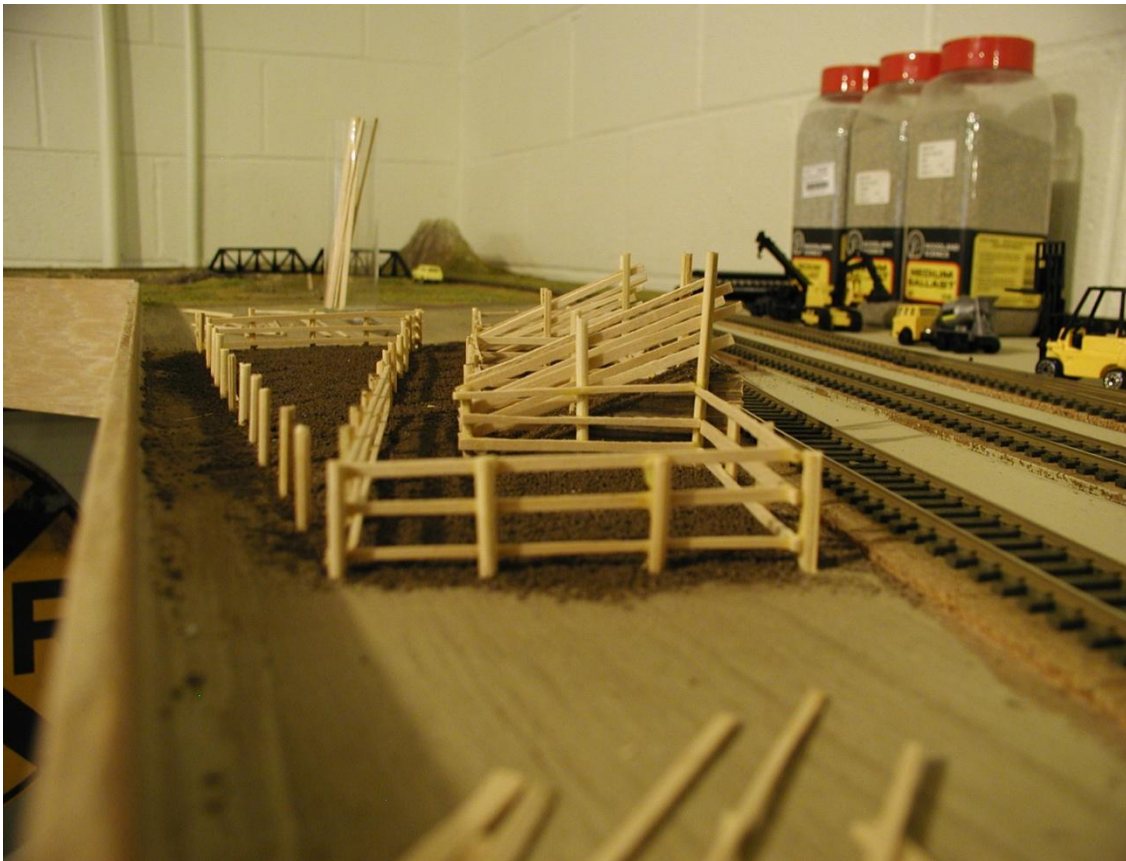
Neal Anderson, Ed Smith and Michele Chance listen as Bob Halsey talks about his involvement in model railroading during a delightful round robin discussion and introduction session.

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## Scenes That Are Low (or no!) Cost Out Of Pocket Building A Cattle Pen

By Neal Anderson

When you have a very small budget to work with, you have to think outside of the box when building models for your layout. I had several cattle cars on the layout with nowhere to put them. I was given some bamboo skewers to use on the railroad and didn't know what to do with them, so I started to make fence posts. I needed a cattle pen, but didn't have the funds to pay for a kit. Using the skewers for the vertical posts, I then cut KFC coffee stirring sticks in half to make the horizontal boards. I picked up some dirt from the yard and put it in the bottom of the cattle pens for looks. So far zero cost to me.



The start of the cattle pens

Next I drilled into the benchwork and set the sticks in, white-glued them, then cut the sticks to the needed height. I cut the stirring sticks in half and glued them to the upright sticks, cutting them to length after all was dry. A little brown paint and water mix colored the sticks perfectly. Using balsa wood that was too large for the HO trains, I carved out the center to make a food trough, and added bales of hay for food. The building was made out of the same material. The roof is card stock. A great scene at no cost.





We-An Cattle was born

After it was all made I moved the track around, but I could not move the pens anywhere. To change the track footprint I made a Gauntlet - that was the best way to keep everything in the operation running a lot smoother.

## Building a Fence

While thinking outside the box, I started to wonder what to do with the one hundred and sixty skewer sticks left over. I needed a place for the cows and pigs to roam the layout and not interfere with the trains. I came up with a thought that if I had space on the layout, that would make an area to hold them. A great farm scene came to life. Looking at the sticks, I thought that they would also be a great way to mass produce fence posts.



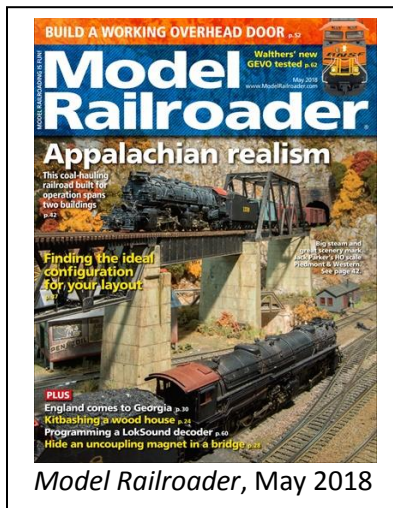


Great for making fences

The fences look like a barbed wire or straight wire fencing. It's just some old thread that my wife didn't want any more. Many scenes were invented at no cost to the layout.



The pigs are happy, and they can't get onto the tracks!



Congratulations again to Jack Parker, MMR, for the appearance of his Piedmont and Western Railroad in the May 2018 issue of *Model Railroader*. Jack's railroad is on the cover of the magazine and represents the feature article for the issue. It's a clear indicator that some fine model railroading is going on in CSD. I briefly operated on the P&W during last fall's picnic. If you'd like the opportunity to see and operate on this railroad, it's up to you. As a benefit to CSD members, Jack hosts operating sessions on his railroad the first Friday and Saturday of every month. Check it out on CSD's [website calendar](#) and make plans to attend. You won't be disappointed.

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## We Need You for RMU 2019

by Doug Algire

Railroad Modeling University (RMU) 2018 is complete. Planning is now under way for RMU 2019 on January 19, 2019. This will be our thirteenth anniversary. The Carolina Southern Division of the NMRA is proud to continue this annual tradition.

We continue to use Christ the King church in south Charlotte as the venue. With the increase in the number of classes we added four class periods in the Multi-Purpose room (MPR). The rooms are more intimate than a regular classroom. Many attendees told me they were better able to see and hear the instructor. Since these were Sunday School rooms and not a high school classroom, we do not have audio-visual equipment for use.

Other comments from students are:

- Great audience engagement by going around the room.
- Very knowledgeable with lots of great tips.
- I appreciated that all the clinics I attended provided handouts. This made my note taking and recall of the topic much easier and more beneficial.
- Need more time.

How can you help? Railroad Modeling University is a team effort. We need volunteers.

- **RMU Dean**. I currently fill this role. RMU Dean has overall responsibility for RMU.
- **Volunteer Coordinator**. Currently Jack Monette is filling this role. Thank you, Jack! He does an outstanding job seeking and getting volunteers.
- People for the White Elephant sale table.
- People for the Division sale table. This could be the same people watching the White Elephant table. Could be more than one person and they can rotate shifts.

- At least two people for the Registration table.
- Four to six people to help setup the night before and teardown at the end of RMU.
- Instructors for each class period.

Without instructors Railroad Modeling University could not exist. The students are appreciative of the knowledge and expertise each instructor displays. Our Division benefits from having instructors volunteering their time and sharing their talents at Railroad Modeling University. The depth, breadth and variety of model and real railroading knowledge is truly exceptional in our Division.

We now have thirty-two (32) class periods available. Sixteen (16) before lunch and sixteen after lunch.

Please consider teaching at Railroad Modeling University 2019. We have members that took a break from teaching. We would love to have you back. Some of you I have spoken to at the monthly Division meeting about coming back to teach. Our classes this year included:

1. Basic Model Railroading
2. Planting Buildings \*\*New Class for 2018
3. Scratch building Cars
4. Open Car Loads
5. Documenting Your Railroad (two sessions) \*\*New Class for 2018
6. Tracks and Wheels
7. Soldering
8. Lighting the Layout
9. Track & Ballast
10. Advanced Ballasting \*\*New Class for 2018
11. Making Scenery (two sessions)
12. LED Lighting
13. Scenicking a Farm \*\*New Class for 2018
14. Reversing Loops \*\*New Class for 2018
15. Light, Sound & Motion \*\*New Class for 2018
16. Background Sound
17. Multi-Function Animation Decoder \*\*New Class for 2018
18. Tips and Tricks from a Master Model Railroader
19. AP Program & Judging (four sessions) \*\*New Class for 2018
20. Module/Sectional (two sessions)





Figure 1 - Fred Miller, MMR, teaching his Light, Sound and Motion Class

Instructors that taught the same class a number of years may think that they no longer need to teach. All of the classes are popular. Of course, some are more popular than others. But hey, who enjoyed all of their high school and college courses?

Recently, prior to the start of a work meeting on the phone the discussion came around to hobbies. I was the only one who said that model trains is one of my hobbies. One the men on the call said, "You PLAY with model trains?" Notice the word play is in capital letters. That is how he said it. My reply was "No I do not PLAY with model trains. I enjoy and operate model trains as close to reality as the full-size trains." His comeback was that it still sounded like playing to him. I proceeded to give him a quick overview of what operation means in the model railroad world.

Our hobby supports whatever we want to do with our model trains. If we want to watch them run in circles we can do that. If we want to move freight and passengers like the full-size version we can do that. "Whatever floats your boat," as the saying goes.

I am sure many of you have run into someone that may have said something similar to you as the man in my meeting. Maybe you took issue with it and maybe not. Typically, the person making the comment has not had the chance to see a layout that supports operation. Now you have an opportunity to let someone in on the fun we have. The education should not be heavy handed nor should we take on a superior attitude. Instead it is a wonderful opportunity to help someone understand what we do and why. Hint, Hint, think RMU. They might become a convert to model railroading or support someone in their family that is a model railroader.

There seems to be three types of model railroaders. Let us take a look at each of them. These are my general observations so feel free to disagree.

**The Arm Chair.** The arm chair model railroader typically does not have any room at all for a layout or even a shelf layout. They support the hobby by subscribing to magazines, purchasing

books, memorabilia, and other related items. They attend local train shows but usually do not travel out of their local area. They usually do not join a club or the NMRA. For them this is enough. They enjoy model railroading.

**The Involver.** The Involver is the largest segment. They support the hobby by subscribing to magazines, purchasing books, memorabilia, rolling stock, locomotives, structures and other related items. They usually have a layout, shelf layout and/or modules. Their layouts/modules are well detailed, look nice and operate well. They attend local train shows and on occasion travel out of their local area to other shows and events. They may join a club, the NMRA or both. When they do join they are active and participate in the club or NMRA activities. They might give a clinic at their club or local NMRA Division. They may run for an office. They enjoy model railroading.

**The Rivet Counter.** The Rivet Counter is just what the name means. They examine every model, structure, and layout for 100% faithful reproduction of the real world. They are sticklers for details on everything. And I mean everything. They do not know the meaning of "just enough is ok". They support the hobby by subscribing to magazines, purchases books, memorabilia, rolling stock, locomotives, structures and other related items. Many have layouts that are finely detailed. They usually join a club, the NMRA or both. When they do join they are very active and participate in the club or NMRA activities. They usually give a clinic at their club, local NMRA Division or national show. Many run for an office both locally and within the NMRA. They enjoy model railroading.



Figure 2- Jack Monnete Scratch Building Class

Please remember these are generalities from my personal observations. You may see yourself a little in each type. Most people will have one type that dominates their model railroading style. None of them are better than the others as each type benefits our hobby. Sometimes, *The Rivet Counter* frustrates *The Arm Chair*. Sometimes, *The Involver* frustrates *The Rivet Counter*, etc. You see where I am going with this? If you are an *Arm Chair* type embrace *The Involver* and *The Rivet Counter* and vice versa. All is good! The bottom line is our hobby has room for everyone no matter what type of model railroader they are. Welcome all of them with open arms. Embrace them, nurture them and you and they will carry our hobby forward. We all enjoy model railroading. These are the people, young and old, male and female that attend Railroad Modeling University. Mark your smart phone calendar now for Saturday January 19, 2019.

Once again, please consider teaching at Railroad Modeling University 2019. We have members that took a break from teaching. We would love to have you back!

We would like to have as many class periods filled by June 30, 2018. This will allow the Division to start advertising at train shows, hobby shops and on-line.

Please contact me at [dalgire@comporium.net](mailto:dalgire@comporium.net) if you would like to continue teaching or have a new class idea.

Happy Railroading!

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## Achievement Program Update

By Neal Anderson, CSD AP Co-Chair

Working on the AP Awards is not that hard to do. The thing that everyone needs to do on some of the AP Awards is to write dates and times down. Some of the main participants just need to fill out the paper work. I can help with the paper work, but I can't do it for you.

- We just had Robert Halsey finish his AP Association Volunteer, (his first AP Certificate!). He is working on his Author Certificate and Golden Spike Award.
- Neal Anderson is working on his Author and Master Builder, Cars.

I would like to start up a column to show the progress of the CSD. Let me know what you are up to so I can fill this column up more. Send me an email or give me a call.

[kklrailroad@yahoo.com](mailto:kklrailroad@yahoo.com) 704-775-2830





## Railroads Under Construction

By Ed Gumphrey

In the past couple of months I have visited four different model railroad layouts in the area, and have spoken with others who have railroads under construction. That's a pretty broad term and some of the railroads that you'll see in coming issues are fully operational and well on the way to being "finished."

I put that word in quotations because we're never really finished, but are always improving things. Even after appearing on the cover of *Model Railroader*, I'm pretty sure that Jack Parker, MMR, will tell you he's still doing upgrades to his railroad. With that said, I'll launch regular updates on model railroads within CSD.

- Jack Monette has a multi-level HO railroad up and operating. He's adding more scenery and details. I'll have pictures and more information next month.
- Anthony Perkins dismantled his last railroad and is building a multi-level HO railroad in a dedicated building behind his house. It features some of the best helix construction I've ever seen. As with Jack Monette's railroad, I'll have pictures and more information soon.
- Andrew Stitt has his expansive HO railroad well along and is conducting regular operating sessions. He's one of the opening stories in this issue as he introduces his choices for a model railroad scheme. As he says, "Be careful what you wish for."
- Superintendent Alan Hardee is well along with his multi-level HO scale Norfolk Southern railroad. About two thirds of his mainline is operational. I'll have some pictures for you next month.
- Ed Smith is modeling the Delaware Division of the Erie in HO scale. It is a point-to-point layout designed for operation. Ed introduces himself in this issue and will whet your appetite for more information with the photo of his basement entrance.
- Tim Rumph is modeling the Southern Railway S-Line in N scale. His situation is one many of us dream about with a full basement and construction modifications to make it ideal for a model railroad. Tim introduces himself in this issue too.
- If planning is part of "under construction," then I have a railroad under construction as well. Inspired by the Baltimore and Ohio during the transition era, I will be modeling in HO scale and hoping to capture the essence of the B&O. I have to finish up some construction projects first, but hope to be starting benchwork this summer. I promise some construction notes along the way.

I'll start our regular updates on railroads under construction with the three following introductory articles. I hope you'll enjoy the regular updates in future issues. Along the way you'll see a range from benchwork to final details. Hopefully my list will grow as other CSD members join in the fun of sharing your work. **If you've got a railroad under construction, let me know.** Send an email to [editor@carolinasouthern.org](mailto:editor@carolinasouthern.org)

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## Be Careful What You Wish For

By Andrew Stitt



Like all model railroaders, I have spent more time thinking about what I would like to do than actually taking the time to work on some aspect of the hobby. A fair amount of time is spent reading about and looking at what others have done. Time is also spent learning about various prototypes - ones that I found to be interesting and ones that later turned out not so much. Going to train shows is tremendous fun - the thrill of the hunt. eBay can also be like eating popcorn - not filling, but a good way to spend some time in search of that special car or structure kit.

When we moved to this area 9 years ago, I struck the perfect deal with my wife. She got the bonus room above the garage for her sewing room in exchange for the full run/control of the basement. Having torn out the previous layout, and having sold the majority of the equipment and structures that went with it, I came down to the basement after we moved in and was amazed at my fortune of the vast open space. Before me lay all sorts of possibilities! What railroad to model or use as inspiration? What scale (I have had layouts from N scale to HOn2-1/2 to HO to Sn3 to ON30 to O)? How big, how small?

I know from having modeled in this range of scales what I am capable of, and what I am not. My hands are not steady enough for delicate scratch building. My eyesight is not good enough for fine details. My patience is not good enough to worry about replicating a prototype exactly. Plus, I really like the thrill of the hunt found at train shows. So, given all of that, after considering N scale, and On30, I chose to return to HO scale.

What prototype to follow? I am not a dyed-in-the-wool follower of any railroad, like some. While I generally lean toward single track, non-major carriers, and short lines, there are also things I like about the big railroads as well. I am also particularly fond of narrow gauge (My dad took me to the East Broad Top for my first train ride!) . Instead, my previous model railroads have tended to follow my "interest at the time." So, in this new basement, what was I going to do? I fell back on an old favorite - the Western Maryland. Something I was familiar with, having gone to Elkins and Cumberland with dad many years ago.

Construction of the layout soon began, with the help of new friends I made in this area. Since my space is roughly 1,200 sq. ft., it took some time to come up with a plan, and operating scheme. Getting to a level suitable for operating occurred in a reasonable amount of time. I am fortunate that some of these new friends like to operate as much as I do, so operating sessions soon began. This culminated with my layout participating in the NC Rail Run. The layout and operating system was as I hoped and expected, and everyone who attended said they had a good time. Except for me!

As I mentioned, after an amount of time (about 2 years) operating the Western Maryland Thomas Subdivision with new friends I made in the area, I was ready to join in the NC Rail Run operating tour. I was pleased to have a full complement of people sign up from a number of states as well as locally. The operating session went off without any major issues, and everyone who attended told me that they had a good time. After it was over, however, I was left feeling let down. I felt that the session was not interesting or challenging enough for me to enjoy going forward.

So, about three months later, after a lot of thought and consideration, I announced to my crew that I would be taking the layout out of service while I made some changes. I was inspired by a railroad map of York, PA I had purchased on eBay. It demonstrated to me that within their city limits they had rail connections in all 4 geographic direction. A lot of railroad for a small area - CSC, Conrail, York Rail, and East Penn. In the era I am modeling, York Rail is now CSX, Conrail still exists, as does the Ma & Pa.



The map that inspired Andrew to change his model railroad scheme. An eBay find, it is an original from the archives of the Western Maryland Railway Historical Society.



In order to model this scheme, I wanted to keep as much of the layout structure in place to make it quicker to get back up and running. I devised a plan inspired by the map, running the line from York to Spring Grove, PA (to reach Glatfelter Paper - a major rail customer). What I found out is that I did not need as much layout as I had built! While I liked the basic form of the WM layout, I did not need as much "open space" as before. Plus an added benefit of shrinking the footprint would be less layout requiring scenery, structures, etc.

So, with the help of a couple of friends (who thought I was crazy for doing this), I cut out a 4' section of layout, and we moved a 20' section over. Later, I removed two other sections entirely, and recycled a third into a new scene. Rewiring the connected sections, and modifying several track sections to fit the scheme allowed me to get back into operating fairly soon after shutdown. With that being said, since then, I have made a number of modifications to sidings and the like to provide a smoother, more logical (to me) operating scheme. As a result of all of this, I gained a reputation among those who run trains with me as "What have you done this time?" I am willing to live with that if the result allows me to be more satisfied with the result.

And that's the bottom line of all of this. Building a model railroad is about enjoying it. If you find out you are not happy with the result, try to determine what you are not happy with, and change it! The end result is something you can learn from and enjoy.

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## Erie Railroad Delaware Division

By Ed Smith



Let me introduce myself. My name is Ed Smith and I have been a NMRA member for 30 years. My wife, Cindy, and I live in Bethlehem, NC, which is across Lake Hickory from Hickory, NC. After early retirement, we moved down here from Northeast Ohio about 11 years ago. We built our house on a sloping, 1-acre lot on the lake, which afforded me a walk-out basement for my dream railroad layout.

Over the last 11 years, because of home construction, obligations of caretaking for both sets of parents, and living in an area with no division affiliation in NMRA, I have been a lone wolf modeler. This has all changed recently. My personal obligations are now gone and

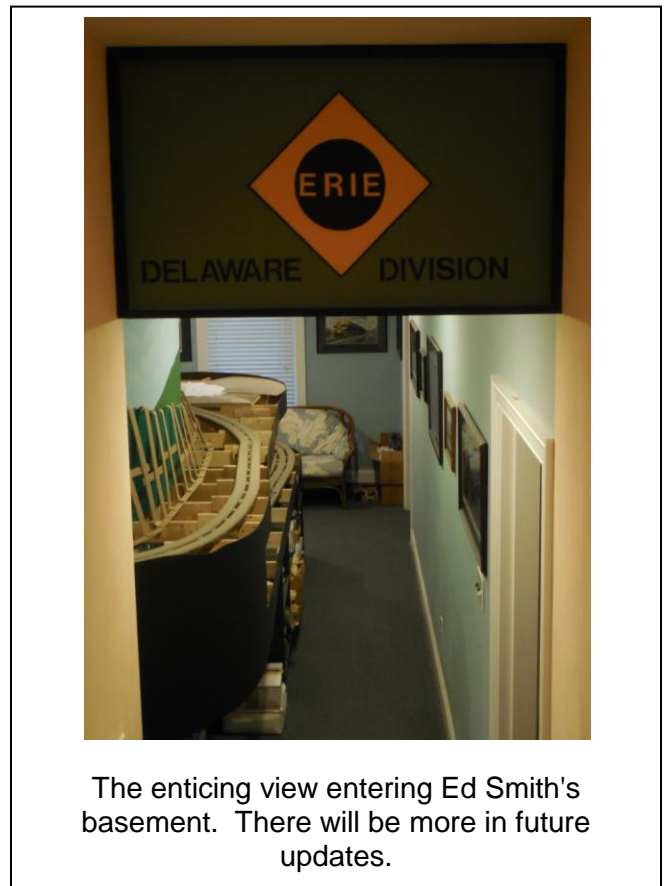
the Carolina Southern division has expanded to include my area. This has encouraged me to be more active in the Division. I had the pleasure of meeting and talking to Alan Hardee and Ed Gumphrey at the Hickory Train Show. Ed informed me he was looking for modelers in the construction phase of their layouts, who may be interested in writing periodic updates in *The Brass Pounder*.

By no means am I an author, but the thought of doing something like this, which is outside my comfort zone, interested me for several reasons. First, it gives me incentive to accomplish construction in a more coordinated fashion; second, our Division will be hosting the Regional Convention in the Fall of 2020, and a somewhat completed layout may be of some interest; and, finally, this may be the incentive I need to jump into the A.P. program. So, I've decided to give it a try.

In this first installment (I hope Ed will permit more), I will give a quick overview of what I'm trying to accomplish. I am modeling the Erie RR, Delaware Division, circa 1947, in HO scale. This runs from Port Jervis, NY (on the NJ border) north to Binghamton, NY. I extended this west to Sayre, PA. This is approximately 110 actual miles.

The layout is a double track main, point to point, on double deck construction, with a helix connecting the 2 decks. This creates about a 700 foot main line run. There will be 3 major cities and yards, 2 interchanges, 2 division junctions, 9 small towns and assorted industries, all prototypical of the area and era. There will be open staging on each level. The layout in its entirety is about 1600 square feet. As of now, the basement is finished, all of the infrastructure is built, and the spline main line and roadbed is installed.

My first update will chronicle the basement and all building up to now, plus my pursuit of attaining the Golden Spike Award. Hopefully, you will at least find something entertaining in my musings and maybe you will find something that may help your endeavors, as I build my layout and pursue A.P. Certificates.



The enticing view entering Ed Smith's basement. There will be more in future updates.



## My Southern S-line

By Tim Rumph

Ed asked me to write a little something for the Brass Pounder, so here goes. My family moved to North Carolina in 1978, while I was in college in the wilds of northern New York. As soon as I got down

here, I fell in love with those big, black engines that were as likely to run long hood forward and with their high short hoods pointed that way.

My family lived in Denver, and the mountains of western NC were an immediate attraction to me. Modeling the S-Line through the mountains didn't start right away though. I started with a free lance "Chevette Sized" layout, 4'3" by 3'6". I even hosted a meeting of the Second Section model railroad club with that layout, but I brought the layout to the Whistle Stop rather than asking everyone to troop out to Denver.

Then life happened. I had a short-lived attempt at modeling the Southern Loops in a spare bedroom of my mobile home, went to engineering school at UNCC, moved to Laurinburg, got married, raised my daughter. Things settled down, and I had an empty 10' x 16' storage building in the back yard. The inside was 9'6" by 14'. I tried to cram the loops in there, but they just didn't fit. I also caught the operating bug, running trains on several layouts in Charlotte, Greensboro, and elsewhere in NC. I started looking at the eastern part of the S-Line, over toward Salisbury/Spencer. I found that not only would this fit better, but the operations were more interesting. I selected 1974, since the Southern was still running the Asheville Special passenger train three days per week. Then I lost my job.

I wound up moving to Lancaster, SC, and found a house 1-1/2 miles from my new job with a drive-in basement. That was in August 2016. I sold the house in Laurinburg, which meant I could remodel the basement, which took all of 2017. Here's the first train on the first section of the new layout.



The First Train on Tim Rumph's Southern S-Line

Yes, the engine's a Jersey Central RS3, but not for long. Still the same piece of the Southern, the S-Line between Salisbury/Spencer and Morganton. Stay tuned and I'll take you with me on my journey with my new layout. By the way, this is N-Scale.



## Division Brass

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